

## TRAFFIC PATTERN PROCEDURES C-182

### 6. ABEAM TOUCHDOWN POINT

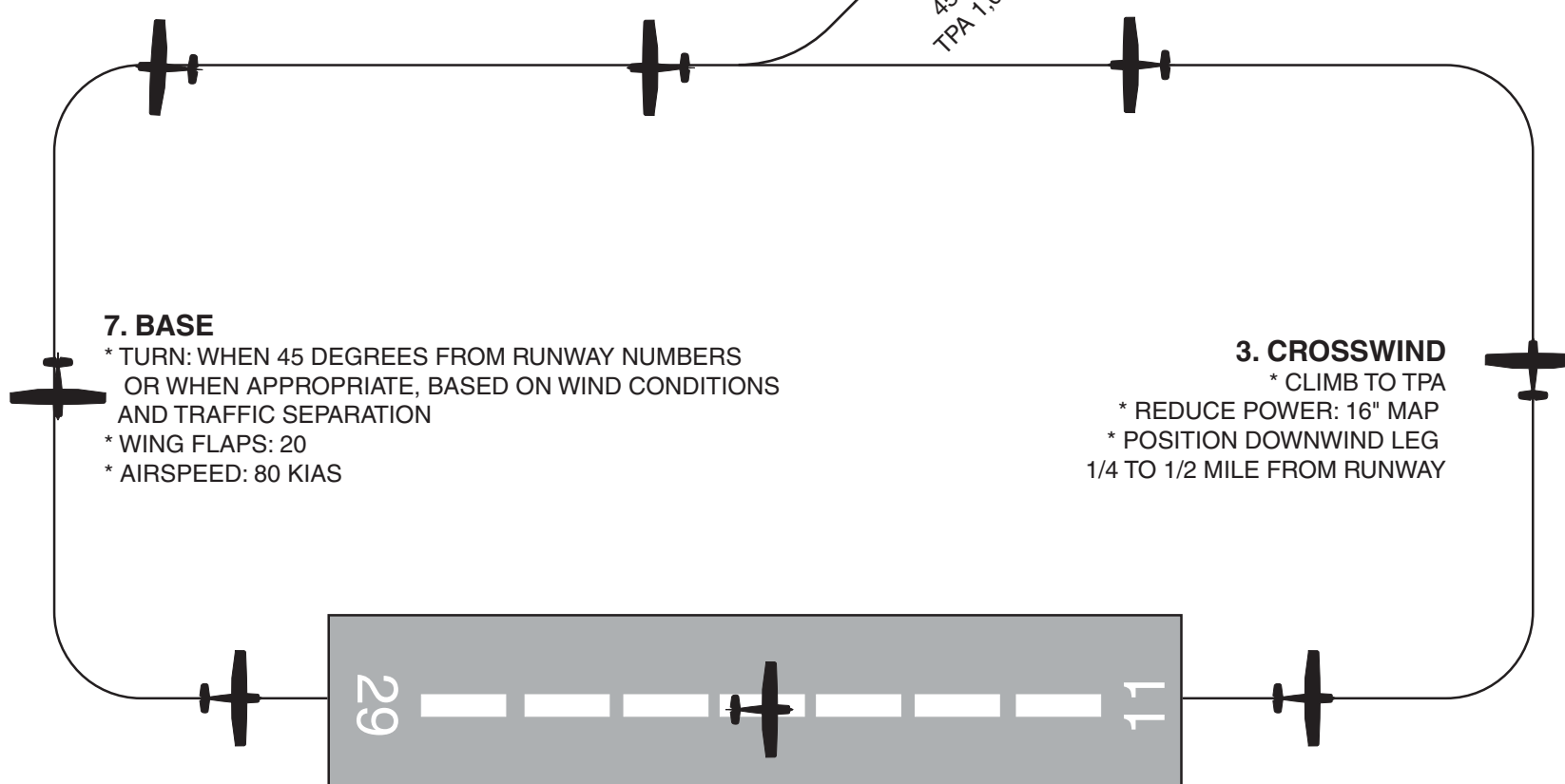
- \* REDUCE POWER: 12" MAP
- \* AIRSPEED: 90 KIAS
- \* WING FLAPS: 10
- \* BEGIN DESCENT

### 5. MIDFIELD DOWNWIND

- \* BEFORE LANDING CHECKLIST (GUMPS)
- \* CALL MIDFIELD

### 4. DOWNWIND

- \* AIRSPEED: 90 KIAS
- \* COWL FLAPS CLOSED



### 7. BASE

- \* TURN: WHEN 45 DEGREES FROM RUNWAY NUMBERS OR WHEN APPROPRIATE, BASED ON WIND CONDITIONS AND TRAFFIC SEPARATION
- \* WING FLAPS: 20
- \* AIRSPEED: 80 KIAS

### 3. CROSSWIND

- \* CLIMB TO TPA
- \* REDUCE POWER: 16" MAP
- \* POSITION DOWNWIND LEG 1/4 TO 1/2 MILE FROM RUNWAY

### 8. FINAL

- \* WING FLAPS: FULL DOWN (IF DESIRED WHEN LANDING ASSURED)
- \* AIRSPEED: 70 KIAS  
60-70 WITH FLAPS - 70-80 NO FLAPS
- \* "GUMPS"

### 1. TAKE-OFF:

- \* WING FLAPS 0-20 (10 RECOMMENDED BY CESSNA)
- \* DG SET TO RUNWAY HDG.
- \* COWL FLAPS OPEN
- \* THROTTLE: FULL OPEN
- \* LIFT - OFF: ESTABLISH Vy

### 2. UPWIND

- \* WING FLAPS: UP (IF REQUIRED)
- \* MAINTAIN EXTENDED RUNWAY CENTERLINE
- \* CLIMB TO WITHIN 300' OF TPA
- \* LEFT CLIMBING TURN TO CROSSWIND LEG (BEYOND DEPARTURE END OF RUNWAY)